

THE NATIONAL MODEL RAILROAD ASSOCIATION



THE NATIONAL MODEL RAILROAD ASSOCIATION was founded in the USA in 1935 for the purpose of developing standards allowing the interchange of equipment from one model railroad to another. It has accomplished this goal admirably and along the way developed into an outstanding international organization offering numerous benefits to its model railroad members. For anyone interested in the hobby of model railroading, we heartily recommend your active membership.

Membership in the NMRA offers the opportunity to develop and expand your modeling skills by association locally with the other NMRA members through the Australasian Region organization, functions and newsletter.

Why you should join!

Reason #1 The Bulletin

Is published monthly in the U.S. From the coloured cover through to the back page, you receive a minimum of 52 pages each month all devoted to the hobby and the prototype.

Typical articles in recent months have included:

- The Case for Practice Buildings
- Naming your Pike
- Motive Power Performance Report
- Simplifying Switching
- Modular Layouts
- Photographing Your Models
- Traction Modelling
- Making Trees
- Lettering & Weathering
- Backdrops

As well there are many regular columns on a variety of RR topics

Reason #2 Special Interest Groups

The NMRA provides the opportunity to correspond with members of approximately 47 separate special interest groups which apart from covering each of the major U.S. Railroads (C & O, D & H, EBT, GN, S.F., S.P. etc) also cover such diverse interests as Circus Modelling, Computers, European Railways, Layout Design, Modular Railroads, LGB, O scale, S scale and Narrow Gauge.

Reason #3 Annual Index

Each year, the Bulletin contains a complete index of all articles published in the American Model Railroad and Railfan Press including Model Railroader, Railroad Model Craftsman, Narrow Gauge & Short Line Gazette and of course, the Bulletin itself.

The NMRA member searching for a particular reference has only one place to look to find that elusive article.

Reason #4 Geographical Listing

The NMRA regularly publishes a listing by geographical location of all 22,000 members as well as an alphabetical cross listing. This means that members have the opportunity to correspond with and visit other members with like interests if they so wish.

Those of us who have had the opportunity to visit North America and the U.K. can tell of the many warm friendships formed in this way. We have also had the pleasure of entertaining overseas visitors who have contacted us in the same manner.

Reason #5 Standards and Data Sheets

The NMRA has formulated most of the standards and recommended practices existing in model railroads today.

These standards mean that the purchaser of better known railroad models or track knows that they will be compatible with his or her other equipment when they get it home.

Standards, Data Sheets and Recommended Practices are available to the membership with Data Sheets being included in the Bulletin as they are released.

Reason #6 Tape Slide Clinics

The Region now has available for rent a variety of clinics on various prototype and model railroading topics. A box of 35 mm colour slides plus an audio cassette is rented to members for a small fee to cover certified mail postage. New clinics will be added as received from the U.S.

Reason #7 The Australasian Region

The Region was created early in 1984 to represent modellers in Australia and New Zealand. It is one of only two regional organisations of the NMRA outside the USA and Canada.

Members reside in all States of Australia and in New Zealand. Membership has already passed the 200 mark and is continuing to increase at a most satisfactory rate.

Whilst catering primarily for modellers of US & Canadian prototype, the Region has modellers of Australian, British & European Railways amongst its members.

The Region publishes a quarterly newsletter titled "Main Line" containing news of interest on the local scene. Members meet regularly at members homes on a rotating basis.

You can join both the National Model Railroad Association and the Australasian Region by completing the application form below and mailing it with your payment to our Epping address.

Membership of the Region costs \$A7 or \$NZ9. As you have to be a member also of the NMRA to be in AR you should also indicate your current NMRA membership number on the form OR add the local equivalent of the \$US20 annual membership fee to your payment - we will then send your money to the US by air-mail so you will quickly start to enjoy the benefits of both NMRA & AR membership. The current (September 1986) amount that needs to be added is \$A33 or \$NZ40.

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Surname _____ Initials _____ Home 'Phone () _____

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AR Annual	\$A 7.00 or \$NZ 9.00
AR 5 years	\$A 28.00 or \$NZ 36.00
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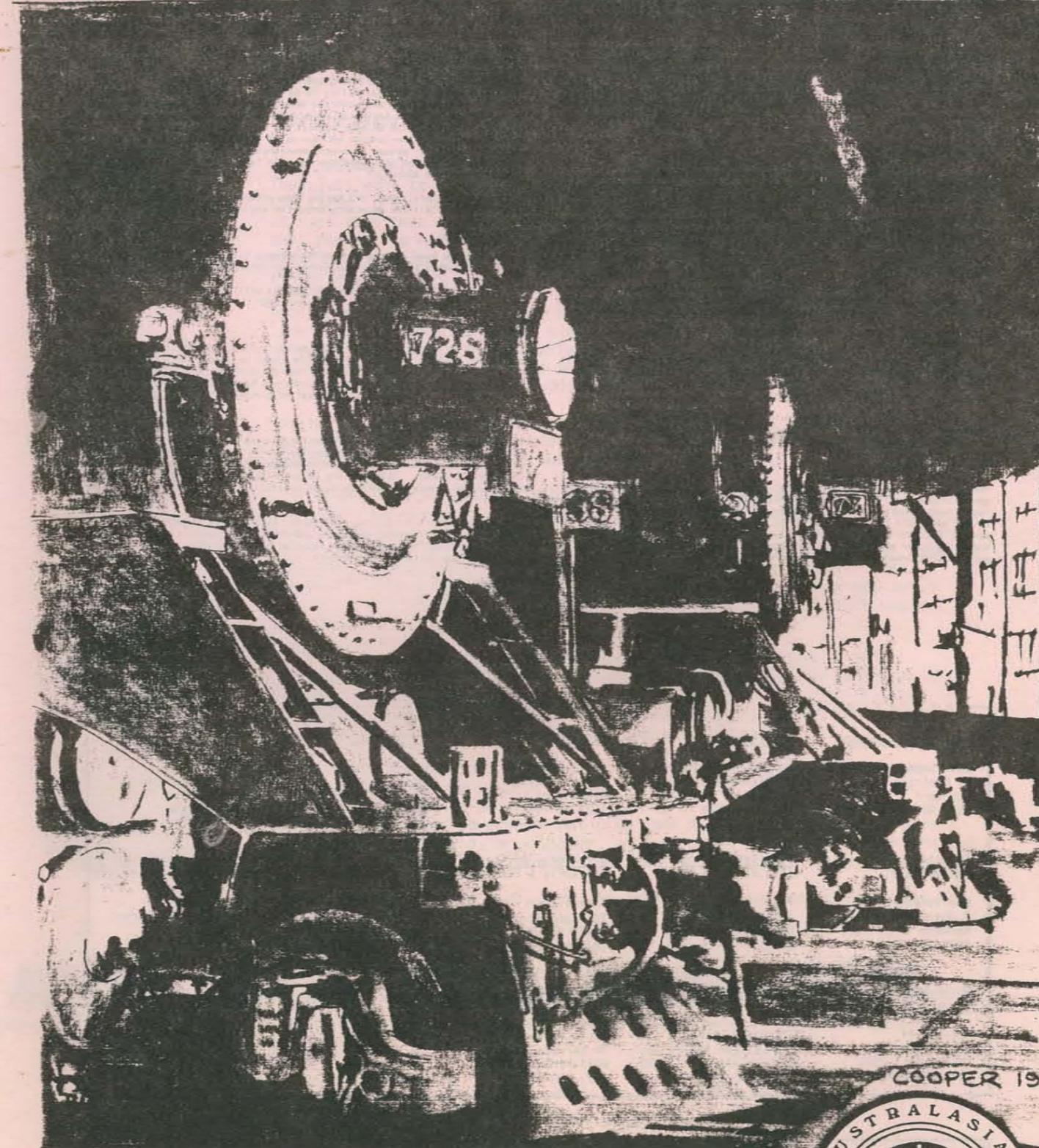
Send to: NMRA - Australasian Region: P.O. Box 412, EPPING NSW 2121

HOW TO JOIN



MAIN LINE

OFFICIAL PUBLICATION OF THE AUSTRALASIAN REGION



VOL 3 NO 3

JULY, AUGUST, SEPTEMBER 1986

EDITOR *Bill Cooper*

BUSINESS ADDRESS

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N.S.W., 2119, Australia.

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FROM THE PRESIDENT

Change of Officers

We now have our new team in place for 1986/1988 when elections are due again and we have had our first meetings to plan future progress for the Region. Please feel free to telephone any of us at our numbers set out below (or write if you like) with your ideas - all will be welcomed and considered - it is YOUR Region, not just that of the Officers.

Newcastle Exhibition

I had the pleasure of again helping the Combined Hunter Region Rail Modellers Association judge the layouts and models at their exhibition held on 9 and 10 August at Newcastle.

Once again it was an excellent presentation of the hobby with a friendly committee out to show they can do it better and in some respects, they are succeeding. Sincerely recommended for a visit next year.

Liverpool Exhibition

Once again with the assistance of AMRA we will be in attendance at the E.G. Whitlam Centre on the October holiday weekend. Come and say hello and purchase an item or two in support of Regional funds.

Membership Fees

We announced a membership fee increase to \$47 at the Convention, the first in two years and to be held for a further two years. Subsequently the Australian dollar depreciated further so that at time of writing \$432.78 is required to pay a \$US20 NMRA membership.

To lessen the impact, we are continuing to charge \$45 only for Regional renewals but this is less than production and mailing costs of Main Line and we won't be able to sustain this for much longer.

Peter Webb's Layout

Many of us visited Peter Webb's excellent S.P. inspired layout in the roof?/garrett?/mezzanine? of his photographic studio in the City of Sydney. I can regretfully report that it is no more due to Peter's move to other premises. He has been able to salvage buildings, some track and switches, control panels, etc. but the realistic rock work and other scenery is no more. Hopefully Peter will find another location to exhibit his talents so we can enjoy his railroad modelling once more.

Cedar Valley Short Lines

Due to a change of house following the departure from home of the last of our four children, the Cedar Valley is no more. I was able to break it up into four large sections and with the help of six stalwart members move it by means of a borrowed table top truck into the garage of the new home. There it forlornly sits on edge against one wall whilst I rack my brains on how to move it (the largest section is 17 ft long and 5 ft wide at one end) down 16 steps, around a corner, down another 16 steps, another corner up 6 steps and then into two rooms (a load-bearing wall to be demolished) via 4 doorways!

Thoughts of M scale, HO_n3, On3 having been eliminated, it looks like some further surgery will be necessary before we are running once again.

Postal Address

Please note we have changed our postal address to P.O. Box 412, Epping, N.S.W., 2121 this being more convenient for collection purposes.

The magazine still will show the old address until renewal of our bulk mailing permit as to change it would cost \$40!

U.S. Ballot

Thank you for the excellent response to the ballot, the best returns on a percentage basis of any Region! Elaine Jones (a lovely and efficient lady) was elected as Western Vice-President representing our Region amongst others. Also the ballot regarding foreign regions was passed so that we are now in every sense a full Region with an equal vote at Trustee meetings.

Annual General Meeting

The AGM was held in conjunction with the Convention in May. Apart from a change to the Constitution to reduce the quorum for executive meetings to three from four, the other main business was to confirm the new officers in their positions. Subsequently Graeme Nitz was appointed to represent Victoria and Tasmania, there being no nomination for that area in hand by the AGM.

Phil Knife and Clive Riley were presented with plaques in appreciation of their efforts as founding secretary and treasurer respectively. The plaque for Fred Gill was provided to him later as he was not able to attend the Convention due to family commitments.

Until next time. - JOHN

AUSTRALASIAN REGION DIRECTORY

President & Trustee	Vice-President	Secretary	Treasurer	Main Line Editor & Tape Slide Despatcher
John Saxon 37 Beatrice Street Clontarf 2093 (02) 949 4767	Bruce Lovett 26 Blaxland Street W. Killara 2071 (02) 498 4659	Garry Wheatley 3 Acacia Place Greystanes 2145 (02) 604 9192	Kevin Brown 33 Tobruk Street St. Marys 2760 (02) 623 7612	Bill Cooper 2 Mason Avenue Cheltenham 2119 (02) 86 1724
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Main Line is the official journal of the Australasian Region of the National Model Railroad Association, Inc. It is published four times per year in approximately February, May, August and November.

Articles, letters, members classified advertisements and club notices are solicited from the membership and are considered to be donated free for the benefit of the hobby. They should be addressed to the Editor at the address shown in the Directory on this page.

Paid advertising is welcomed. Current rates payable in advance are \$100 for a full page for four issues, \$50 for a half page and \$25 for a quarter page.



« EDITORIAL.. »

OPERATION?.. yes/no*

* Delete inapplicable item

Some people HATE operation.

They just want to run trains, without any of that tedious rigmarole with car cards, train orders and obscure regulations. Operation seems to attract more than its fair share of nit-pickers, too. You'd have to be one, really, to wade through the mire of practices involved. Still, there must be something in it, as some pretty switched-on people seem to like it. Wonder why?....

Operation implies that a model railway is run in a full-size manner, involving some form of schedule, either time or sequence-based. Passenger trains are composed and timed prototypically, with an origin and destination; freight trains are often dealt with on an individual car basis, with flows of empty and loaded cars to and from industries.

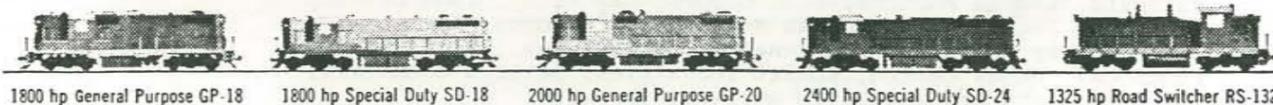
Of course, some subterfuge is necessary, to disguise the incredibly tiny operating world. Hidden staging loops; fiddle yards and so on, with subtle (or unsubtle) compression of the visible parts.

Trains are fascinating, but the administrative procedures behind their operation becomes sophisticated, complex and labour-intensive. Obviously, running the full-size is no barrel of laughs; nor too, apparently are there many on exhibition layouts, judging by the intent, unsmiling mein of some operators. Especially on those exquisite English-style layouts. The complex of signalling; mystic bell-codes and, of course, those god-awful couplings! (Fishing in the dark between cars with a crochet hook would wipe the smile off a saint)

There is a lot to learn about operation, with a fair amount of skill required in its execution. In the model form there are some unprototypical aspects ... block switching, for example ... but technological advances in control and computer interfacing may relieve this, to let us get on with the good bits. But, is it worth it? Emphatically, YES.

Whether hand-made or out of boxes, just running equipment without rhyme or reason ... or even worse, not running it at all ... is like a musician perpetually tuning an instrument or leaving it in its case. Be it Stradivarius or fiddle, it needs to produce music to come ALIVE. When taking part in a symphony, or rock band, or whatever, an instrument becomes more than the sum of its parts, as does any properly applied equipment.

Operation is just like that, too. An intense pleasure awaits those who try ... and master it.



On the cover..... from a photo by Doug Colquhoun
Classic shot from 1954, in Mile End roundhouse, South Aust.,
with second-generation Webb SUPER POWER in decline. The near
Berkshire has SP-type valancing, missing on the far one.

THE FIRST REGIONAL CONVENTION

As many of you know our first attempt at a small convention went off reasonably well. There were one or two hiccups but overall the reaction of those attending was very favourable.

Whilst the venue (Macquarie University) was excellent we suffered from not being able to get entry to set up displays and tables, etc. until 8.30 a.m. on the Saturday. Thanks to the great efforts of the convention committee however we were largely able to start accepting registrations by 9.15 a.m. and we commenced on schedule at 10.00 a.m.

Our clinic presenters did a great job - I'm sorry I didn't have more time to attend all of them. Next occasion we will allow a little more flexibility in timetable for comfort breaks, etc. and to avoid a repeat of our experience this time when our late arrival for lunch caused much suffering!

Dave Watkins' Junction Hobbies did great business with many items not usually seen in other shops. Dave generously donated a Athearn diesel as door prize.

The model competition was popular and we were treated to some excellent work. Phil Knife, Gerry Hopkins and Ian McFarlane were most meticulous as judges and all models were assessed according to NMRA rules. I'm sure we who entered learnt a lot from the judges assessment of our work and I for one plan to try harder next time.

Then we had Ray Walters photographic display and Sn3 module rushed together for the Convention. Narrow gauge was also represented by Garry Norwood's H0n3 display layout subsequently purchased by Mark DeHavilland and who supplied the video on soldering seen during the show.

The silent auction, under Clive Riley's supervision, was a great success for those who had items to sell and also returned the Region a good commission on sales. There were also films and videos, NMRA supplies for sale and a bus tour for some of the ladies to the Windsor area.

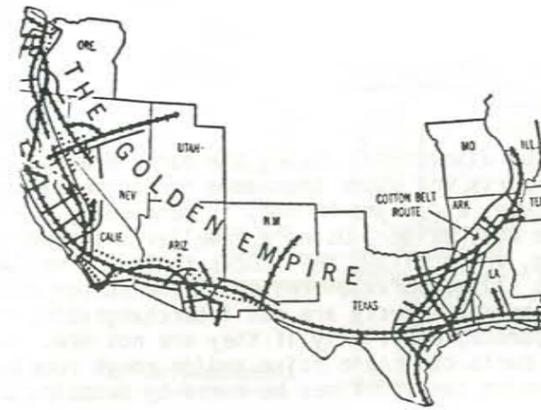
Finally, a good meal at the Black Stump on the Saturday night for about 70 attendees and then a busy layout tour schedule on the Sunday completed a great week-end.

Overall, we made a profit on the Convention of around \$180 which might not seem much for all the work that went into it. However, for our first effort I guess it isn't too bad a result and the funds will help keep our membership fees down.

We couldn't have succeeded without all the help and assistance provided by the membership and others. Apart from those already mentioned I must also publicly record the Region's appreciation to the following in particular although on the day there were many others who lent a hand here and there -

- Bruce Lovett, Brian Nickless, Kevin Brown, Bob Benson, Laurie McLean, Bruce & Tanya Penman, Peter Burrows, Chris Hitchins, Lauris Hopkins, Bill Cooper, Kay Riley, Jane Lovett, Ken Grey, Peter Betts, Warren McLean, Geoff Notts, Kathy Knife, Garry Wheatley, Ian Venables, Toni Saxon, Bergs Hobbies, Punchbow] Hobby Centre, Robset Industries, Selleys Chemicals, British Paints and Macquarie University - whew!

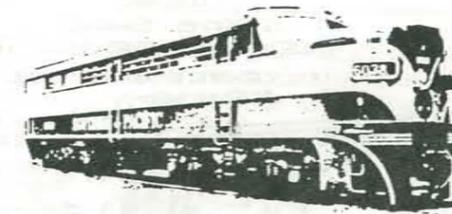
We plan to hold our next Convention in May 1988 again at Macquarie. Start planning your model entries now!



Southern Pacific

NEW WHEELS for a PA

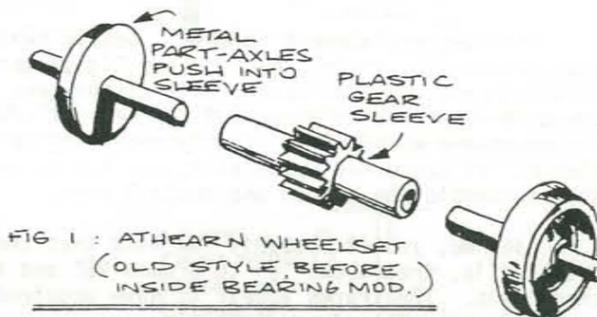
by BILL COOPER



During the rebuilding of an ALCO PA diesel (honorary steam locomotive) by Athearn, the opportunity was taken to fit a set of NORTHWEST SHORTLINE replacement wheelsets. These were favourably reviewed in the May '84 MR, but all was not revealed.

There isn't much wrong with the original Athearn wheels. They have a fine profile and ride well on small rail. Being sintered iron, (metal powder squeezed into the shape of a wheel) on steel shafts, they should last forever. Failings include:

- the occasional bad set - wobbly, out-of-round or chipped flange/tread;
- wrong colour - dark grey, rather than shiny steel on tread and flange;
- prone to become dirty - due microscopic pitting resulting from the sintering process.
- 42" diameter - most EMD and ALCO wheels are 40" - some Baldwin and FM diesels had 42".



As good replacements are readily available from Athearn dealers, as the wheels are not hard to clean (See Fig. 2) and as a period of operation imparts a realistic graphite-like sheen to treads, none of these 'failings' are hard to live with. Nevertheless, having bought the NWSL wheelset kit, I determined to use it.

First the locomotive must be dismantled. Using the parts drawing packed with the loco, account for all parts and store them in a compartmentalised box, like those used for fishing tackle, etc. The PA body is removed by pulling outwards on the skirt. Put the body aside. Using a jeweller's screwdriver, prise off the worm gear housing, which allows the trucks to fall free (work over a bench so that they don't fall too far). Prise off the bottom cover plate and the trucks will fall apart. Trucks are not interchangeable, front to rear, so keep assemblies separate, especially if they are not new. With use, parts acquire a set and mixing parts can cause noise and/or rough running. Conversely, noise and rough running can sometimes be cured by swapping parts about.

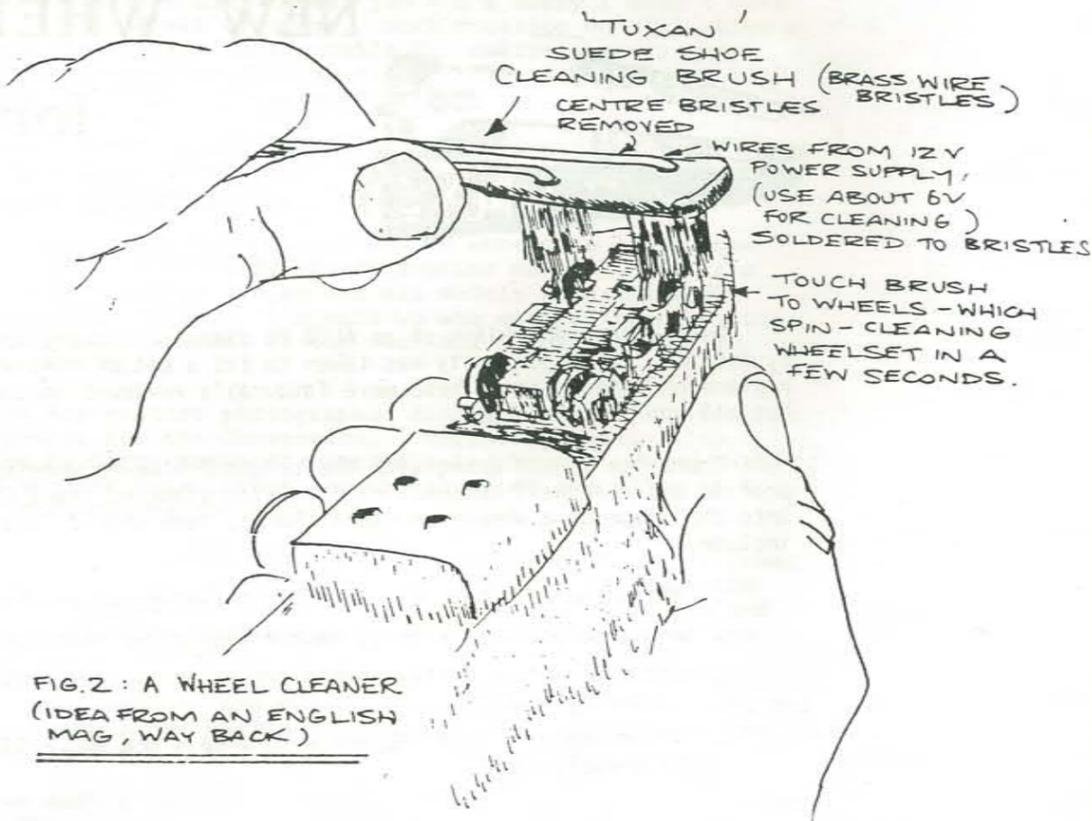


FIG. 2: A WHEEL CLEANER (IDEA FROM AN ENGLISH MAG, WAY BACK)

Wheelset replacement is easy. Gentle twists and pulls on the original wheelset reduces it to 3 parts; (see Fig 1) gentle twists and pushes with the new wheels and original geared axle sleeve sees a complete wheelset once more. Use an NMRA standards gauge (of course!) at several points on flange, to check for gauge and wobble. It is only necessary to approximately centre the geared sleeve. If concerned about slip, use ACC to secure the axle-sleeve junctions. Now reassemble the trucks and install them.

Like me, you'll probably now find that the minimum radius has risen drastically, from about 18" to around 26" and that the loco lifts through checkrails. The trucks appear to have acquired a too-rigid wheelbase, caused by a reduction in axle sideplay.

Dismantle the trucks once again. The reduction in lateral play does no harm in the end axles, as it makes the loco track more accurately, PROVIDING there is adequate play in the centre axle. This can be achieved by removing the centre bearing flanges.

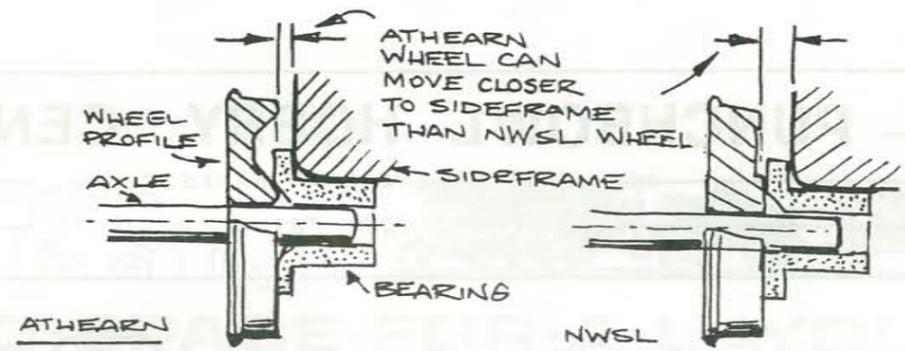


FIG. 3: COMPARISON OF WHEEL CROSSSECTION

Using a small jewellers' screwdriver, prise out the sintered bronze centre axle bearings. Work carefully at several spots, as they are a tight fit. Working at one spot on their flange can result in a broken, distorted bearing, jammed in the sideframe Now put them in a small vise, secured by their flange (see Fig. 4). Cut off the flange with a jewellers' or Exacto saw. Clean up the cut edge with a file, then replace the bearings in the sideframes. Now ... PRESTO! ... the trucks glide through check rails and a reasonable maximum radius has been restored.

Considering that both Athearn and NWSL wheelsets meet NMRA Standards, what was the problem? It seems to have been in the wheel crosssection (see Fig. 3) The Athearn wheel's undercut section simply allows more sideplay than the more rectangular NWSL section.

An incidental advantage of the wheelswap was that the 1" height reduction so gained really is noticeable, giving the PA an even longer, lower, sexier look than before. Incredible.

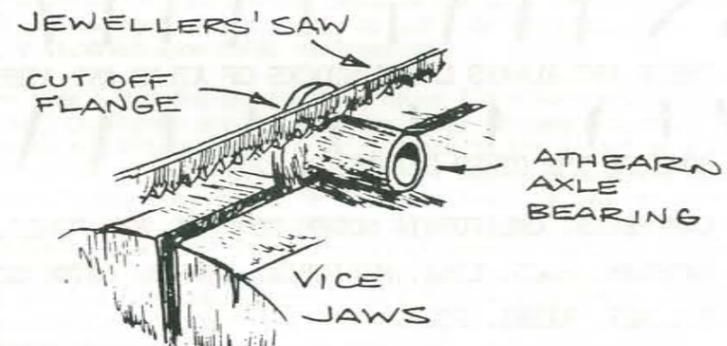
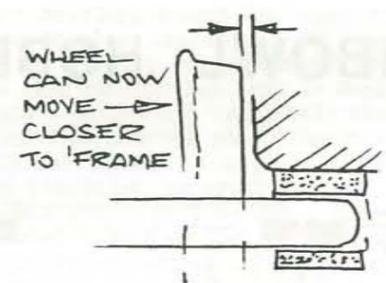


FIG. 4: CENTRE AXLE BEARING MOD.



A version of MURPHY'S LAW* states that 'projects are always more difficult, expensive and take longer than first thought'. So it proved with the PA, about which an extensive construction article is being put together. Keep tuned.

* 'If anything can go wrong, it will', attributed by Douglas Aircraft designer Ed Heinemann (responsible for AD Skyraider, A4D Skyray; A4 Skyhawk) to a WWII US Navy procurement officer named Murphy, who formed and actively used his famous law in assessing carrier aircraft designs.

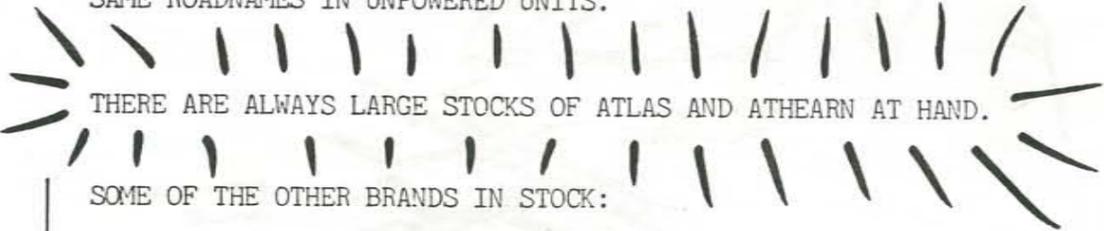
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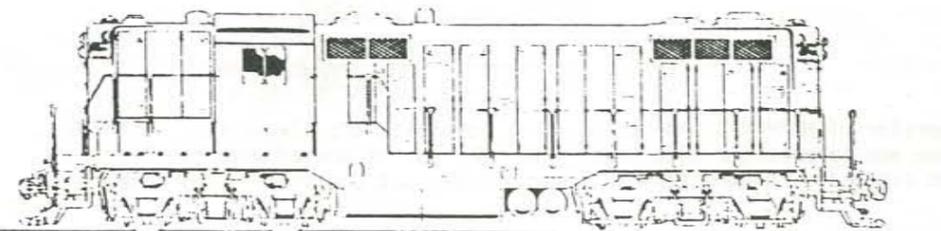


SOME OF THE OTHER BRANDS IN STOCK:

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SO COME ON IN AND BROWSE AROUND AT

PUNCHBOWL HOBBY CENTRE



NO-SPACE-FOR-A-LAYOUT SYNDROME

By Lawrence Nagy

I've always been a victim of the No - Space - For - A - Layout Syndrome ever since I first became interested in trains way back in early high school. Despite this handicap I've always had a layout of sorts.

Back in those embarrassing days when I used to freely mix U.S. and European N gauge equipment, I slept in a 1500 by 3300 bedroom. It contained a bed, a wardrobe and a study table, with no room left over to swing a cat - even a baby Manx (they don't have tails). Not knowing any better at the time, I just went ahead and built a layout, totally oblivious to the fact I had NSFAL. All I knew was that the stuff I'd bought represented the best part of two years beer bottle collecting (I collected empty ones then - now I empty full ones) and I was going to have a layout no matter what. The previously mentioned wardrobe was about 1200 high. Upon this I rested a 900 x 1500 plywood sheet which became the main sceniked part of the layout. Two tentacles of track over a period of time snaked themselves around the walls on shelves between 30 wide in front of the window to 150 wide elsewhere. Thus, totally by necessity, I stumbled upon NSFAL railroading.

By the time I married and bought a house I'd discovered Model Railroader, Railroad Model Craftsman and books by the likes of John Armstrong. Though I had considered the alternatives, I still stuck to the layout on a high shelf concept for consecutive layouts, even where I had more room available. My current Pennsylvania New England is in half a smallish two-car garage; which in fact holds the wife's car, my motorcycle, assorted lawn mowers, weed eaters, gardening tools and other devices of weekend torture, timber storage, workshop, pushbikes and cockroaches. Although the latter don't take up much room, it's still a formidable amount piled into a 5000 x 5000 area. Especially when you consider the layout is a medium large mainline system which services seven separate towns with twenty car freights. Yes, I know it's N scale, but it's still a lot of layout.

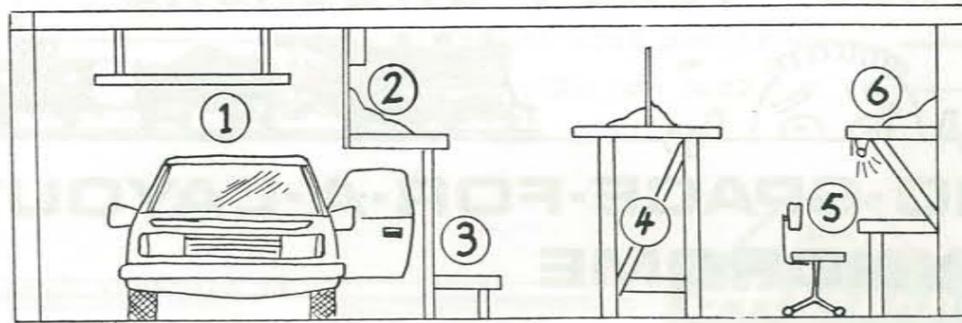
Two physical considerations made such an extensive system possible in such a small area. They are layout height and the means of support.

The layout height revolves around two opposing factors. How high does the layout need to be to clear obstacles, and how low does it need to be to still be accessible for viewing and operation? You'll have to experiment for your individual requirements but keep in mind that a few centimetres of difference in a person's height can completely change their view of the layout. If you're taller than average your friends might not be able to comfortably view and operate the layout. Also, the higher the layout becomes, the narrower the shelves should be, as reach is drastically shortened by height. A true eye level layout should only be 300 - 400 wide in comparison to the 1000 or so a waist level layout can be. A foot stool or step might be the solution to elevate visiting midgets or to enable you to reach hard-to-get-at areas.



CHICAGO & EASTERN ILLINOIS RAILROAD

332 South Michigan Avenue, Chicago 4, Illinois



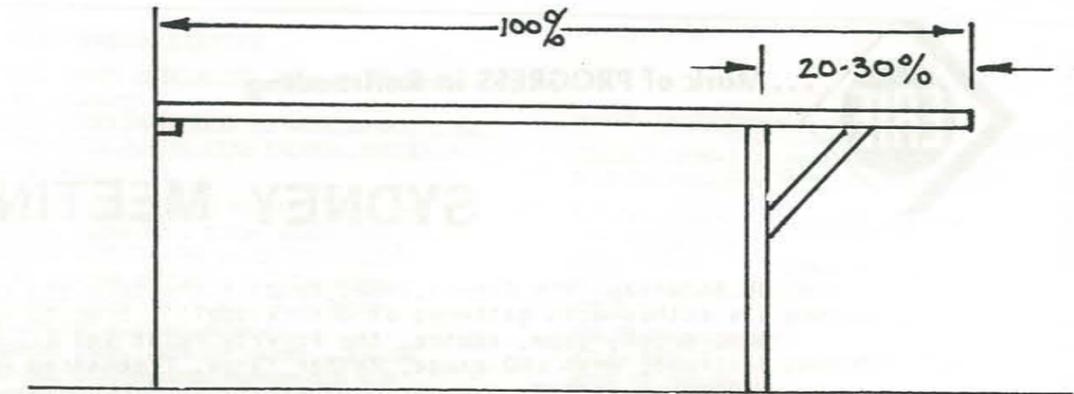
KEY TO DIAGRAM

1. Family conveyance, with timber storage over. Door opens under layout to improve access.
2. Rear of benchwork hangs from ceiling, front section supported by legs on floor. Note that all legs are set back from the edge of the benchwork to leave toe room.
3. Strategically placed foot stools allow more reach/viewing in busy areas. Not necessary everywhere, only in front of towns, yards and other switching spots.
4. Sway bracing essential for narrow/high peninsulas. Placed near the ends of peninsulas, legs allow open space under for storage etc. See fig. 2.
5. Work area under layout. Work lights suspended under layout. Chair stored under 4 when not in use.
6. Wide shelves diagonally braced to walls to improve rigidity.

So, if we ignore the traditional waist height at which to build the benchwork, we open up new areas above work benches, the family car or whatever. If you own a Rangerover you might have to settle for a short span over the bonnet, as the roof's a wee bit high to consider building over; but the traditional Holden or Rice Bubble can be spanned without too much trouble. In my case, the layout clears the car doors on the passenger side to make passenger access easier. See the diagram.

The other criterion, that of support, is equally important. No point in having all that room available under the layout if it's filled with masses of timber shoring. Anchoring the shelves solely to the walls is ideal for this reason, but some legs are going to be needed for peninsula areas and extra bracing under wide sections. Don't rule out support from the ceiling either, as shown in the diagram. It's practical when the supports are located behind the backdrop, if that can't be arranged then the supports will have to be disguised by mountains or grain silos. John Allen used these tricks a few times on the Goore and Daphetid - and if you get the chance, study photos of the layout as every trick known to man (and then some) was used to expand the Gorre and Daphetid.

UNION PACIFIC RAILROAD
OMAHA, NEBRASKA 68102



ELEVATION OF PENINSULA

By bracketing one end of the peninsula directly to wall and placing the legs 20-30% of total length back from end, the best compromise of clear span, strength and underside space is realised.

A true shelf should probably be less than 450 wide, while a diagonally braced shelf can approach 1000, or even go beyond if light weight scenery is used. One aspect to keep in mind is that with high layouts any peninsula areas tend to be unstable unless the legs are bolted to the floor or built out of full sized railway sleepers. It's comparatively easy to dodge benchwork when it's waist high, but when it's shoulder high you're always bumping into it, so stability is important. Watch out when you pick things up off the floor - when you go to straighten up your head will be travelling at half the speed of light when it finally connects with the bench-work. As well as having an impressive collection of lumps on your cranium, you'll also have the dirt about repeatedly rerailing equipment. Coming to think of it, those impromptu earthquakes might be just the thing for a Southern Pacific layout set in California!

Here's a short list of things I've had to consider when planning to build my layouts 1500mm (5ft) off the deck -

Viewing - realistic perspective make narrow shelf construction easy to disguise, but large vistas (assuming you have room for them anyway) are impossible.

Construction - track and scenery construction requires the use of foot stools but oh! the joys of accessible wiring!

Operation - hand thrown turnouts and manual uncoupling are difficult without foot stools. Same with reading reporting marks on cars behind other cars.

Trackplan - tight curves aren't as obvious. Hidden tracks under the layout are far more accessible.

Guests - if you're over six metric feet tall spare a thought for those with duck's disease. Try not to smile too broadly when kids unsuccessfully try to get their mits on the trains.

In summary: if space is a problem, consider going over the obstacles instead of around them. A bit of mind bending and unorthodox benchwork might be just the solution you need to get those longer trains rolling further.

Why don't you tell us about problems and solutions you encountered in constructing your layout? If you don't, you'll have to suffer another one of my articles next newsletter



...Mark of PROGRESS in Railroading

SYDNEY MEETING ♦♦

On Saturday, 9th August, 1986, despite the cold and rain, thirty six enthusiasts gathered at John Kiddell's home to stand in awe, drool, gape, admire, the Poverty Point and Blood Creek Railroad, John's HO gauge, rather large, freelanced American prototype layout.

We were treated to long trains, all steam powered, running through breathtaking mountain scenery, some of which reached almost to the ceiling.

With 4.5 scale miles (300 actual feet) of single track mainline, the trains took some time to traverse the layout. I am sure some of the members were playing a "guess the spot where the train will re-appear", which added to the enjoyment.

After a couple of hours of spectacular running, we were treated to a veritable feast by John's charming wife, Beverly, assisted by her sister and daughter-in-law. Thank you ladies.

With the inner man satisfied, Bruce Lovett forced on the suffering members, a slide show, half of which was his beloved Spokane, Portland and Seattle Railway, then slides of various model layouts.

After the slide show, while he still had a captive audience, he apologised for the absence of John Saxon, who had his arm twisted into being a judge at the Newcastle Model Railway Exhibition.

He covered other items such as the slight increase in dual membership to \$37.50 per year, due to our Keating dollar, the N.M.R.A. stand at the Liverpool exhibition on 4th, 5th 6th October, where we require help in manning the stand and finally, Bill Cooper needs articles, drawings etc. for our Mainline magazine.

When I left at 6.00 p.m., there still were sounds of steam trains straining to lift their loads over the summits.

John and Bev, on behalf of the members, thank you for a most enjoyable meeting.

S.P. & S.

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H. F. ENO, Passenger Traffic Manager



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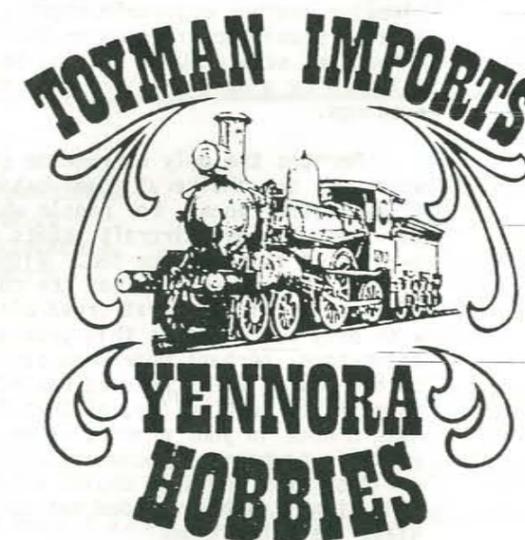
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* * * * *

CANBERRA CLAN ♦♦

Despite all the political rumblings, threats, innuendo, mud slinging, emanating from the big white house on the shores of Lake Burley Griffin, the modelling scene is more one of quiet progress, with reports from clubs, groups and individuals in the area beavering away in the evenings and weekends (one good thing about the ACT's climate is that it is not conducive to outdoor activities in winter.)

Our most recent meeting was held at Stephen O'Brien's home where we took in a video of the N & W J. Class Northern. Great viewing, much enjoyed by Steve (being a N & W modeller) - the video provided by Kerry McPherson. Likewise another enjoyable night was had at Ian Macfarlane's home where, after a session of running on the "Central", we then admired Ian's latest efforts of scratchbuilt Rolling Stock of Queensland prototype in "S_n3", followed by a bull session. All in all, very interesting and informative evenings.

Perhaps the only retrograde step one can report from our neck of the woods was the "Malkara" model exhibition. From our hobbies point of view it was rather poor. For people who are more into collecting Corgi, Dinky toys, etc., model aircraft, boats and plastic models, then it would have been worth the entrance fee. Without boring people with local politics, perhaps it is enough to say the current organisers do not look upon model railway layouts as great crowd pleasers. Plus they take up too much space! With only four layouts this year including the one by the Hornby tinplate collectors, perhaps modellers can look forward to March when bigger and better things are scheduled for this area. For instance, six layouts, three never seen before, of British, Australian, NSWGR, USA and Japan. Make a note in your diaries or on your dunny wall! It will be worth the visit to Canberra. Incidentally, March is still warm in this part of the world so skis and snow chains will not be required. It is also hoped an NMRA stand can be provided but our numbers will be spread very thin to stage the exhibition.

Next Meetings:

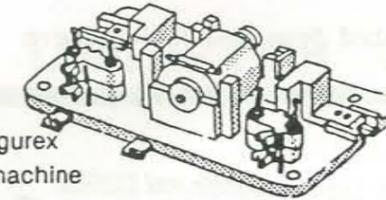
PETER WELLER-LEWIS

<u>SEPTEMBER</u> Tues 30	P Weller-Lewis 2 Hayley Close, Queanbeyan.
<u>OCTOBER</u> Tues 28	J Bailey 11 Charlton Street, Pearce ACT.
<u>NOVEMBER</u> Tues 25	K McPherson 21 Crossley Close, Melba ACT.
<u>DECEMBER</u>	To be advised



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MELBOURNE MEETINGS

1986

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Oct 3	Robert Williams, 43 Margaret St., Kilsyth	-
Nov 7	Robert Dall, 7 Murchison Ave., Vermont Sth.	221.6034
Dec 5	Neville Scantlebury, 239 Whitehorse Rd., Balwyn	817.2238



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- Compiled by Bill Vinikour & re-printed from the Mid-Eastern Region's "Local"

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LOADS FOR COAL AND ORE CARS

by Lawrence Nagy

In the April 1986 *Model Railroader*, page 109, Tony Koester described how he used crushed coal for loads in coal cars (*hoppers*, to you non-Pennsy types). Loose coal was simply poured into the car until it was full. The idea was to quickly and simply weigh the car down for better operation. Tony remarked that derailments cause a big mess which needs cleaning up.

I've used a similar system for years with N scale cars. My mainline grades tend to be steep, so completely filling my cars to the brim with coal made them too heavy, especially if lighter cars are run in the same trains as the coal cars. The idea of cleaning up spilled coal after detailments seemed a bit too prototypical for my taste, so here's my solution:

- Cut a plug of foam or a sheet of cardboard to create a false base about 2/3 high in the car;
- Carefully pour in the coal to the desired height and profile;
- Spray on wetting agent with an atomiser, eye dropper or straw;
- Pour on diluted white glue, a drop at a time, to preserve the pile's shape.

When it dries, you'll have a hard crust of coal holding the loose stuff down. If you have ever used the bonded ballast method on your layout, you'll already be familiar with the technique.

The cars tend to be top heavy, but so far I've had no derailment problems with them - in fact, they track far better than empty hoppers, *touch wood*.

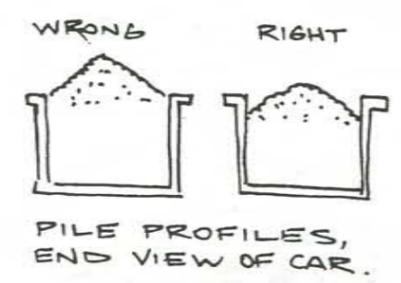
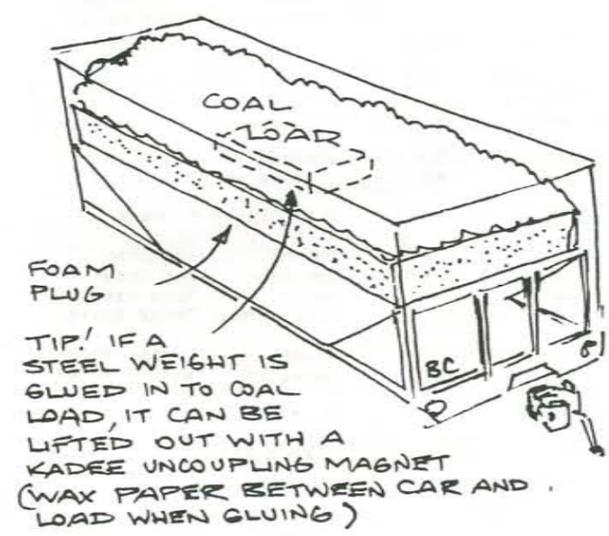


FIG. 1 : LOADS FOR COAL AND ORE CARS

AWARD WINNERS

There were 66 entries in the competition this year, the first to be held by our Region and the first to be held under the auspices of the NMRA Achievement Award Program. Entries were placed in all nine categories, although one (Diesel Locomotives) attracted only one entry and no award was given.

Probably more important for many modellers than winning awards was the achievement of the minimum 87.5 points for inclusion in the Achievement Program. Those who did, but did not win a place award, all received a Certificate of Commendation. The contest was judged strictly under the NMRA rules and points awarded will be recognized by the national body.

As the chief judge, and a veteran of many competitions in this country, I can only comment most favourably on the very high standard of modelling exhibited. Arguably, this event represents the highest modelling standards seen anywhere in Australia so far. While some of the top prizes went to non members of NMRA, this can only do good for the Region by spurring us all on to achieving even higher standards in our models. Perhaps next time we members can out-achieve the outsiders at their own game! The whole purpose of a competition is to encourage even higher standards in our own modelling, and this is further reinforced by the NMRA Achievement Program.

Before listing the award winners, I would like to record my great appreciation of the work of my fellow judges, Gerry Hopkins and Ian Macfarlane, whose tireless efforts throughout the whole day of the Convention ensured that the results were available by the end of the day. My sincere thanks and appreciation go also to Kevin Brown and Bob Benson, our competition clerks and recorders. None of us expected such an overwhelming response to the competition in the number of entries received, and without the dedicated help of those two gentlemen throughout the whole day we would never have got the judging done.

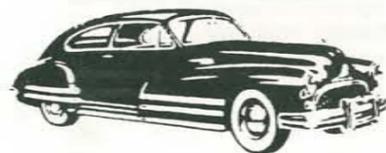
Now for the awards. In each category we were able to award a First Place and Runner Up and, as mentioned above, Honourable Mentions were awarded for excellent models that did not make the top two. That this was necessary indicates the very high standard of modelling exhibited overall. Total points available are 125, of which 87.5 are needed for inclusion in the Achievement Program (NMRA members only).

AWARD WINNERS

Category	Award	Points	Model	Winner
Steam Loco	First	116.5	LSWR 4-4-0	Peter Betts
	Second	112.5	MR 4-2-2	Peter Betts
	HM	90.5	Shay	W. Turnbull
	HM	89.0	212 4-4-0	Phil Badger
Diesel Loco	No competition			
Passenger Cars	First	103.0	LSWR 4-wh	Peter Betts
	Second	76.5	Combine	Hal Saxon
Freight Cars	First	99.5	Conoco Tank	G. Norwood
	Second	94.0	Horsebox	Peter Bette
	HM	90.5	Cement Car	Eddy Garde
Cabooses	First	89.0	Drovers' Van	Peter Betts
	Second	85.0	Logging Van	P. Morrow
Non Revenue	HM	76.5	Pile Driver	R. Casey
Structures	First	104.5	Totton Sta.	Ken Gray
	Second	95.0	Cottage	Ken Gray
	HM	94.0	Depot	John Saxon
	HM	90.5	Signal Box	Peter Betts
Displays	First	102.0	Sawmill	Laurie McLean
	Second	100.5	Derelect	Warren McLean
Traction	First	94.5	E. M. U.	Bill Cooper
Best in Show		99.5	Conoco Tank	G. Norwood
(awarded to NMRA members only)				



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80-20	Optimum Use of Space	71	56
80-22	Scratch Building From the Prototype	81	21
80-23	Scenery and Layout Photography	104	60
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80-31	A Look at Model Railway Techniques	90	60
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80-34	Distinctive Rolling Stock	86	33
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80-56	"Kitbashing - A Modelling Concept"	52	23
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